



City Council I-345 Briefing

Feasibility Study

From I-30 to Woodall Rodgers Freeway (Spur 366)

October 2022

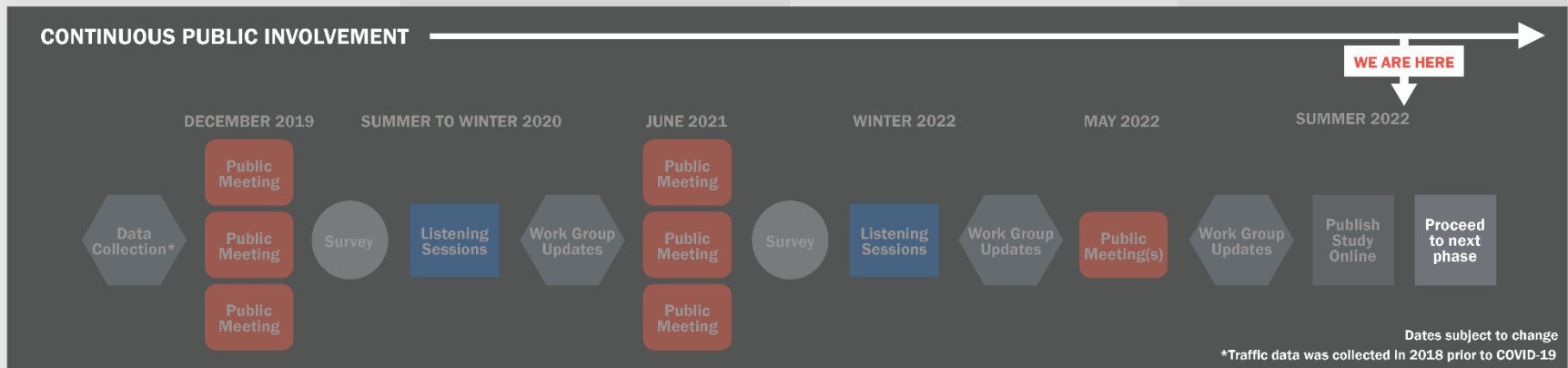
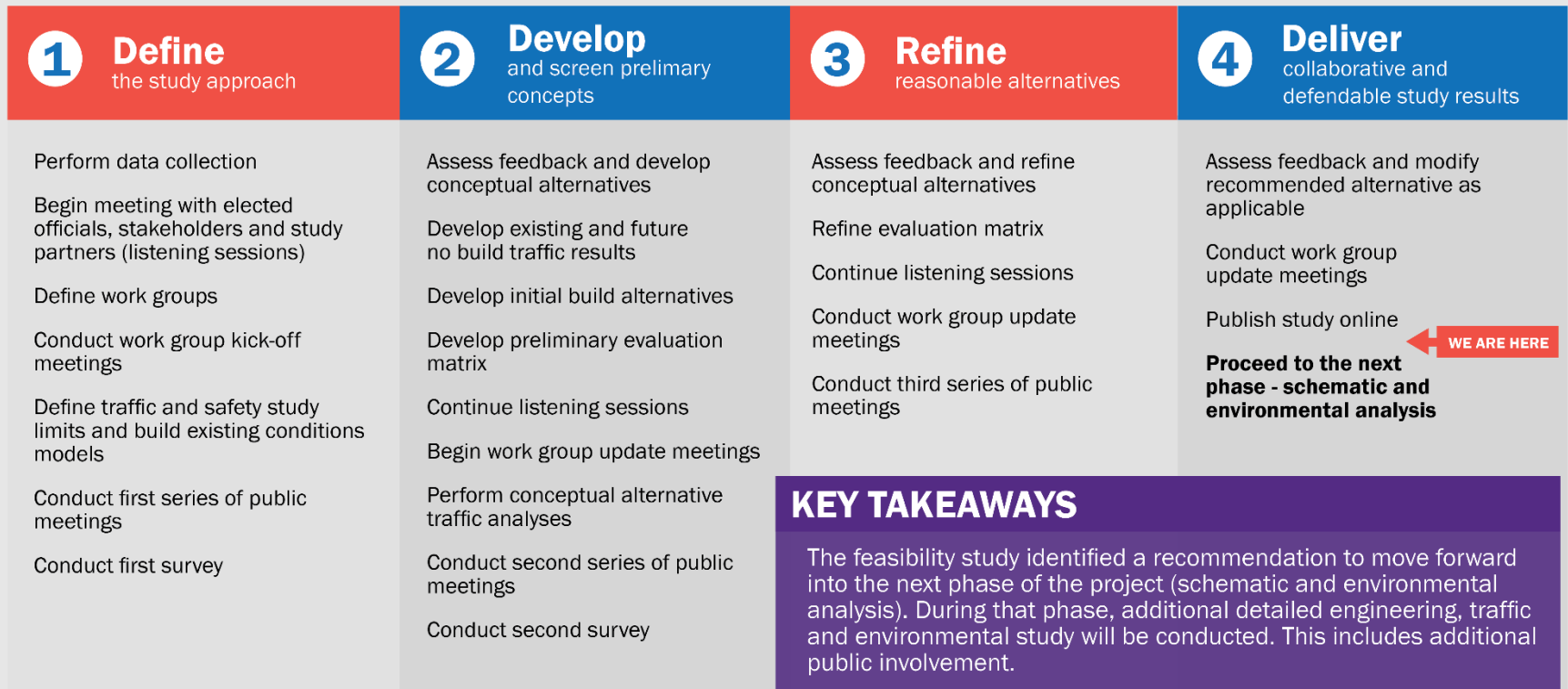
Dallas County, Texas



- I-345 begins at I-30 and ends at Woodall Rodgers Freeway (Spur 366)
- Most people don't realize that they are on I-345 because it is not signed



Study Approach and Timeline





CityMAP Goals

Presented at May
2022 public meetings

- Mobility
- Connectivity
- Sustainability
- Economic Development



I-345 Feasibility Study Goals

- Carry forward CityMAP Goals of Mobility, Connectivity, Sustainability and Economic Development
- Have an inclusive, transparent and collaborative public involvement process
- Work collaboratively with stakeholders
- Review recommendations from previous studies
- Provide the best solution that maintains safety, mobility and operability
- Defendable results
- Incorporate TxDOT and community goals
- Work towards recommended alternative



Why Study I-345?

As Dallas County population continues to grow and I-345 reaches its estimated remaining useful service life, it is necessary to plan for the future of the roadway. This study will help to determine the future of I-345.

Previous Public Involvement (December 2019 and June 2021)



PUBLIC MEETING SERIES 1

1362

of surveys received

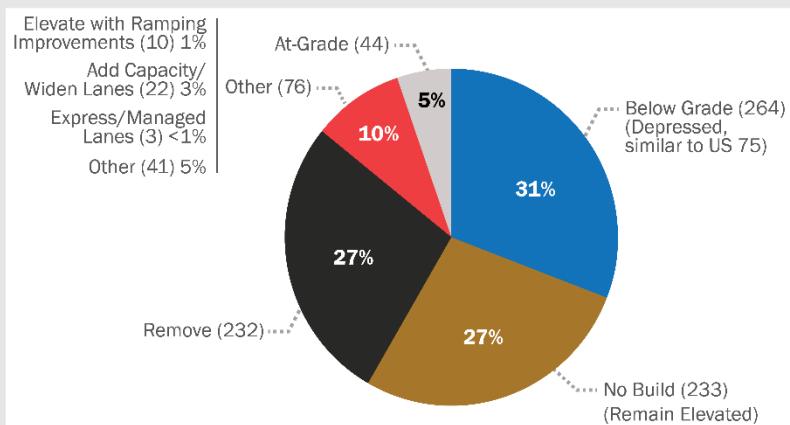
130

of written comments received

15

of verbal comments received

PREFERRED ALTERNATIVES



Total Responses: 849

KEY TAKEAWAY

There was a pretty even three-way split for public preference on alternatives to evaluate in the feasibility study. More than 70% of respondents suggested further analysis of the alternatives presented in the 2016 CityMAP Study.

PUBLIC MEETING SERIES 2

1023

of surveys received

175

of comments received via comment form

47

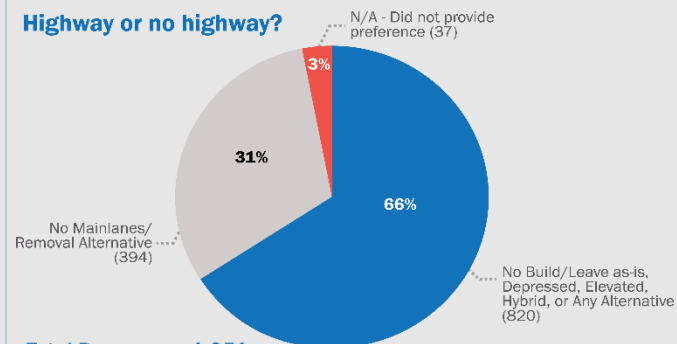
of voicemail comments received

6

of email comments received

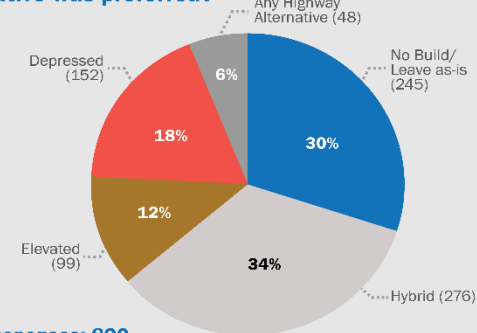
INPUT RECEIVED FROM SURVEY ON ALL ALTERNATIVES*

Highway or no highway?



Total Responses: 1,251

Out of the 66% (820) above that want a highway, which alternative was preferred?



Total Responses: 820

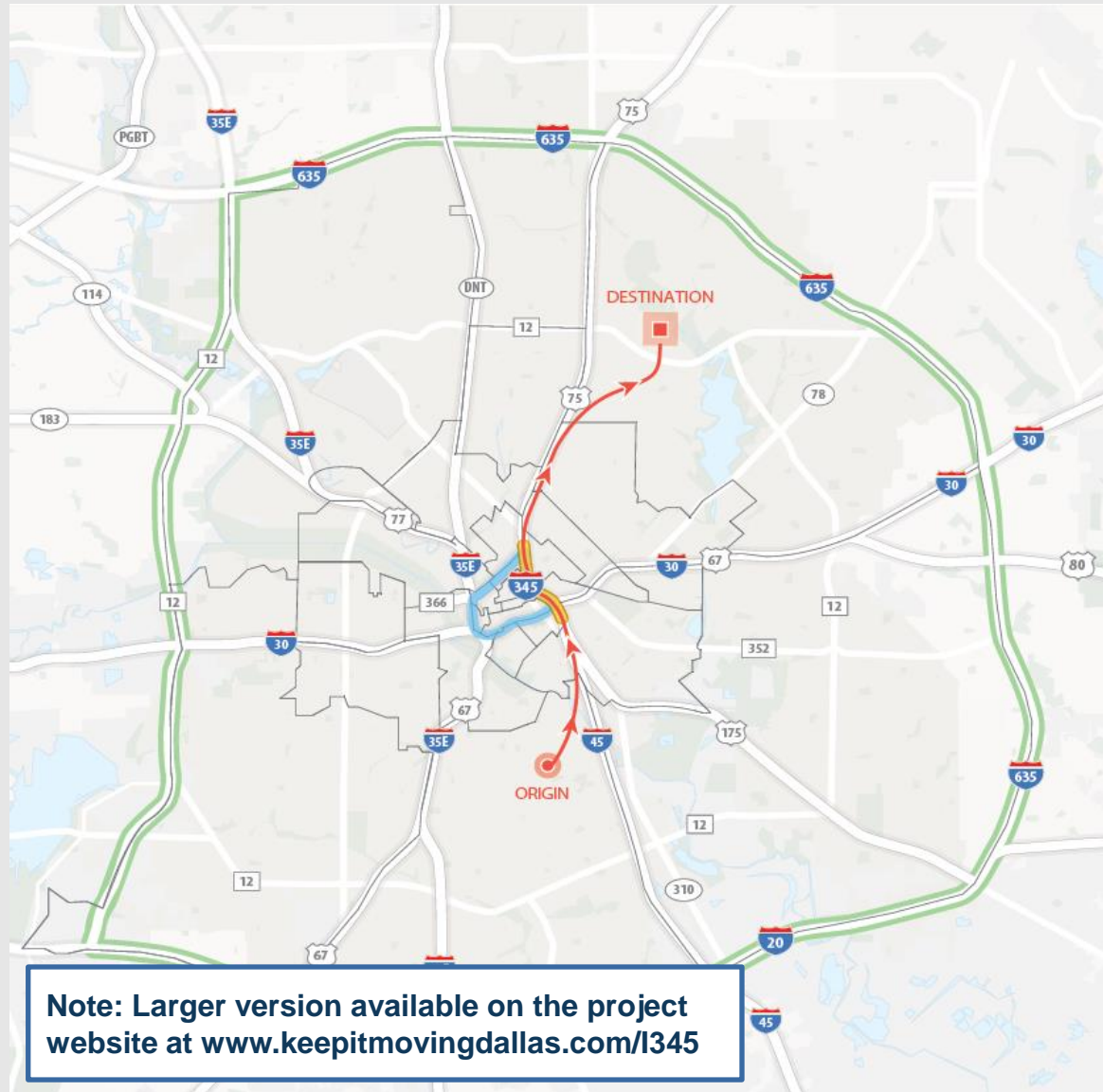
*Includes input received through the SCOUT survey, comment form, online comment form (surveyMonkey.com), email, and verbal comments

KEY TAKEAWAY

More than 65% of respondents stated that they preferred a highway alternative versus a removal alternative. Of those that preferred a highway alternative, over 50% of respondents stated that they preferred a below grade (depressed/hybrid) alternative.



- Traffic counts collected in 2018 (prior to pandemic).
- A key to evaluating the alternatives is to understand the travel patterns of current users of I-345 within the study area, and into and out of the study area.
- The information is not limited to the I-345 study limits.



Origin & Destination Distribution - Thru Traffic Northbound on I-345

Presented at May 2022 Public Meetings



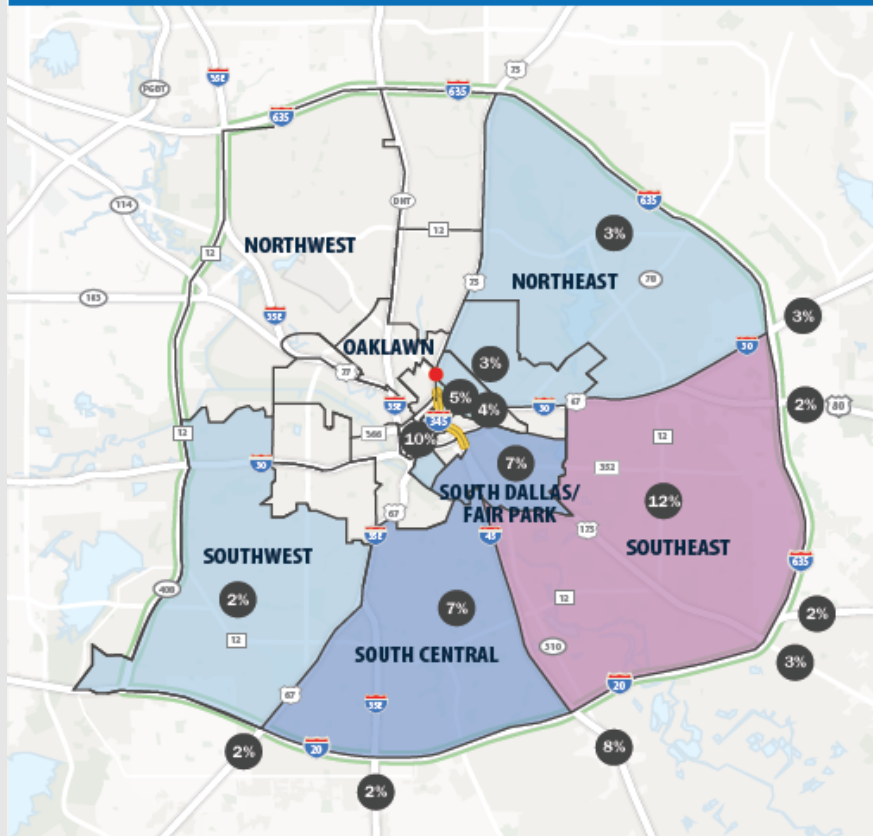
Key Takeaway

Daily* traffic using I-345 northbound mainlanes is originating from the southern half of Dallas and is destined to the northern half of Dallas.

The distribution shown is approximately 75% of the daily traffic traveling northbound on I-345. Approximately 7% of the remaining traffic originates within the study area in multiple zones with small distributions. Approximately 18% of the traffic originates outside the study area through other roadways (minor arterials) not collected in the data.

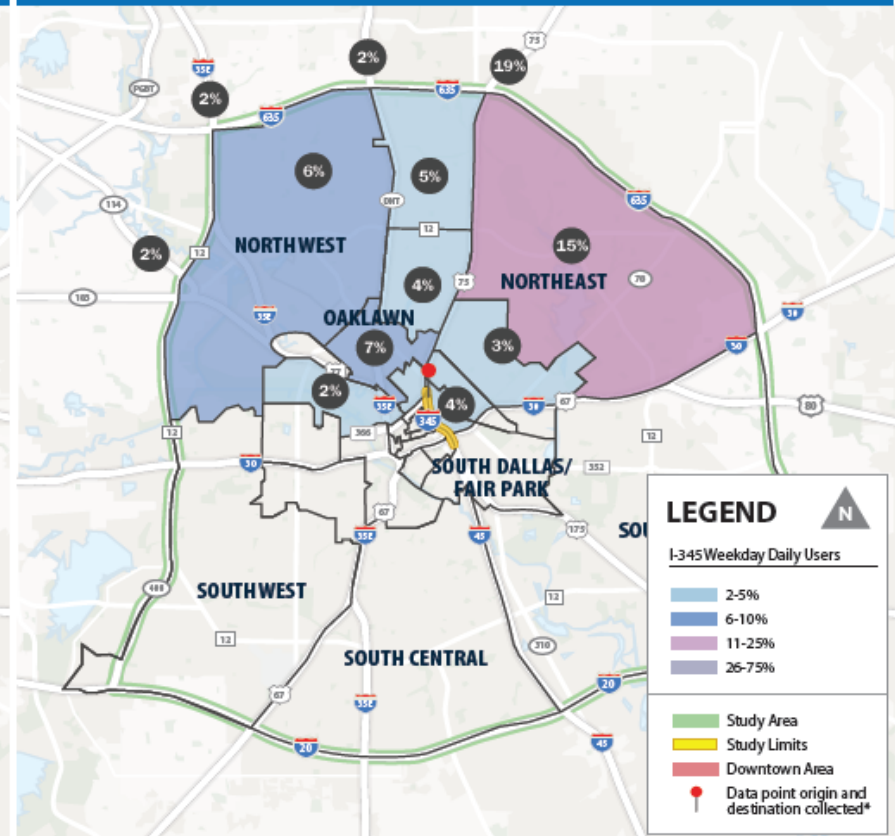
The distribution shown is approximately 71% of the daily traffic traveling northbound on I-345. Approximately 14% of the remaining traffic is disbursed within the study area in multiple zones with small distributions. Approximately 15% of the traffic leaves the study area through other roadways (minor arterials) not collected in the data.

ORIGIN MAP

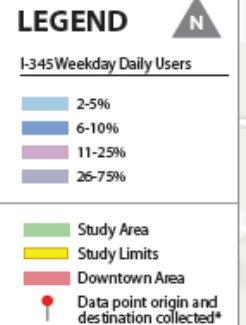


This is the same information presented at the June 2021 public meetings.

DESTINATION MAP



*Volume at the data point is approximately 83,000 northbound trips a day



Alternative Evaluation Matrix

Presented at May
2022 public meetings



*Note: No new ROW would be required with any of the proposed alternatives. This includes no impacts to natural resources (wetlands, streams, farmland, wooded areas or floodplains) or cemeteries.

**N/A = Not applicable

Criteria Rating Scale in comparison to the No Build/Leave I-345 As-Is

Does not achieve criteria	Sometimes meets criteria	Neutral/No Change	Mostly meets criteria	Highly meets criteria

Note: Larger version available on the project website at www.keepitmovingdallas.com/I345

Criterion		Objective	No Build/Leave I-345 As-Is	Depressed	Removal	Elevated	Hybrid	Key Takeaway
Mobility	Vehicles	Minimize impacts to corridor mobility on the freeways and local roads						Due to the changes in access with each proposed build alternative, traffic patterns will change traffic volumes on various freeways and local roads.
	Bicycle/Pedestrian	Improve bicycle/pedestrian mobility						All proposed build alternatives would improve bicycle and pedestrian mobility.
	Transit	Accommodate existing transit facilities and known future proposed transit projects						All proposed build alternatives would accommodate existing transit and the proposed DART D2 alignment. The Removal alternative would have an at-grade crossing with the existing transit facility because of the increased traffic on local roads. With the Removal alternative, DART might have to consider grade separations to improve transit and vehicle operations and safety.
Connectivity	Access between freeways	Freeway to freeway connections						The Depressed, Elevated and Hybrid alternatives maintain the I-345 freeway system between I-30 and Woodall Rodgers Freeway (Spur 366). The Removal alternative severs the freeway connection.
	Access between freeways and local roads	Freeway to local road connections						I-345 has 16 existing access points (ramps). The Depressed alternative maintains 13 of the 16 access points. The Removal alternative severs the connection of I-345 to local roads. The Elevated alternative maintains 7 and the Hybrid alternative maintains 9 of the 16 access points.
	Access between local roads	Local road connections						In all proposed build alternatives, no new connections are proposed, however, the Taylor Street connection is severed. The Depressed alternative, in addition to Taylor Street, severs Canton Street and Good Latimer Expressway. The Removal alternative, in addition to Taylor Street, severs Canton Street.
	Bicycle/Pedestrian	Improve bicycle/pedestrian facility connections						All proposed build alternatives improve bicycle and pedestrian connections along proposed cross streets or frontage roads where applicable. The Depressed alternative does not maintain a connection across Good Latimer Expressway on the southern end of the study limits.
Sustainability	Agency Coordination	Respond to City of Dallas design guidance and DART D2 future plans						The alternatives were coordinated with the City of Dallas, NCTCOG and DART. The Hybrid alternative is the only proposed build alternative that meets all of the criteria received to date.
	Right of Way (ROW)*	Avoid additional ROW* and displacements	N/A**					All proposed build alternatives avoid additional ROW and would not result in any displacements.
	Parks outside State ROW	Avoid impacts to parks, recreational areas, and public usage facilities like parking, including existing and future amenities, outside existing State ROW	N/A					No additional ROW would be required and there would be no impacts to parks or recreational areas located outside of State ROW.
	Parks and public usage inside State ROW	Avoid impacts to parks, recreational areas, and public usage facilities like parking, including existing and future amenities within existing State ROW	N/A					The Elevated alternative would not result in permanent impacts to the existing public facilities within State ROW. The Depressed, Removal and Hybrid alternatives would result in permanent impacts to public facilities within the State ROW, including Julius Schepps Park, Bark Park Central, and Carpenter Park extension and existing parking lots.
	Communities	Minimize impacts to existing adjacent communities (Downtown/Deep Ellum)						The No Build/Leave I-345 As-Is alternative is perceived as a barrier between Downtown and Deep Ellum. The Depressed and Hybrid alternatives would depress the mainlanes and improve the local road connections at grade, including adjacent bicycle and pedestrian accommodations. The Removal alternative replaces the existing highway with local streets, including adjacent bicycle and pedestrian accommodations. The Elevated alternative would be similar to the No Build/Leave I-345 As-Is alternative, but when reconstructed would allow for better connectivity under the mainlanes, including bicycle and pedestrian accommodations.
		Minimize impacts to existing communities beyond downtown						The No Build/Leave I-345 As-Is, Depressed, Elevated and Hybrid alternatives maintain the connection from South Dallas to North Dallas. The Removal alternative removes the connection and the communities would have to adjust travel patterns to alternate routes.
	Sustainable Design	Minimize maintenance costs through sustainable design elements						The No Build/Leave I-345 As-Is alternative requires significant maintenance to extend the life of the existing structure. The Removal alternative would have the least maintenance costs being an at-grade solution but will increase maintenance on local roads due to the increase in traffic volumes on the local roads. The Elevated alternative would have maintenance costs to inspect and repair any structural deficiencies over time. The Depressed and Hybrid alternatives could have significant maintenance costs to accommodate current DART D2, which requires storm water detention and a pump station. Any potential capping could also add maintenance costs dependent on the type of proposed amenities (TBD).
	Potential Surplus ROW	Amount of potential surplus ROW that could result in development (to be determined) (in acres)	N/A					All of the proposed build alternatives have potential for surplus ROW.
	Economic Development	Property Values Impacts						All of the proposed build alternatives have potential to increase property values at buildout; however, increased property values could result in higher property taxes which may negatively affect some residents and businesses.
		Property Tax Revenue Impacts						All of the proposed build alternatives have potential to result in annual incremental property tax revenue at buildout; however increased property taxes could negatively affect some residents and businesses.
		Potential Cap Locations						Ratings include both surplus ROW and potential development on top of the freeway.
Construction Cost	Cost (\$)	Preliminary, approximate construction cost (2020 dollars)	N/A	\$\$\$	\$	\$\$	\$\$\$	It is estimated that the cost of the alternatives would be approximately: depressed, \$18; elevated \$650M; removal, \$400M; and hybrid, \$1B. There is significant cost associated with the Depressed and Hybrid alternatives. The higher cost is associated with depressing the highway and relocation of existing utilities.



TOPICS THAT MATTERED TO YOU

Common themes from second series of public meetings



Community Cohesion

Better connections to areas east and west of I-345, and potential areas for capping



Traffic Concerns



Impacts to access between South and Southern Dallas and North Dallas



Pedestrian Safety

Economic Development Potential



Potential surplus right of way and areas for capping



NB

No Build/ Leave I-345 As-Is

The existing bridge can only be maintained for so long to stay safe and operational. The cost to maintain the existing bridge will continue to increase over time. Eventually it will become too costly to maintain and replacement will be needed.

D

Depressed Alternative

Severing Good Latimer Expressway and Canton Street does not meet the City of Dallas Design Guidelines and is not favorable by the position papers received from stakeholders.

R

Removal Alternative

The impacts to regional traffic with the removal alternative are significant. Based on public feedback, this option was eliminated to continue to provide a connection of mainlanes between south and southern Dallas and north Dallas.

EI

Elevated Alternative

The existing elevated highway is perceived as a barrier between communities. While the proposed elevated has a smaller footprint and could be built back different, the alternative has been eliminated to provide better community cohesion.

H

Hybrid Alternative

This alternative is the best compromise to combine elements from the other alternatives based on public feedback. Based on input, changes have been made to the hybrid alternative to develop refinements to what is now the “recommended alternative”.



Allows for strategic decking/air-right development opportunities in a below grade/trench configuration, consistent with the City of Dallas Design Guidelines



Provides 10' shared use paths for pedestrian/bicycle access/with safety lighting



Maintains Good Latimer Expy. and Canton St.



66% of survey participants want some type of highway for I-345



Provides mainlanes to connect South and Southern Dallas and North Dallas



Negligible impacts to regional mobility



LEGEND

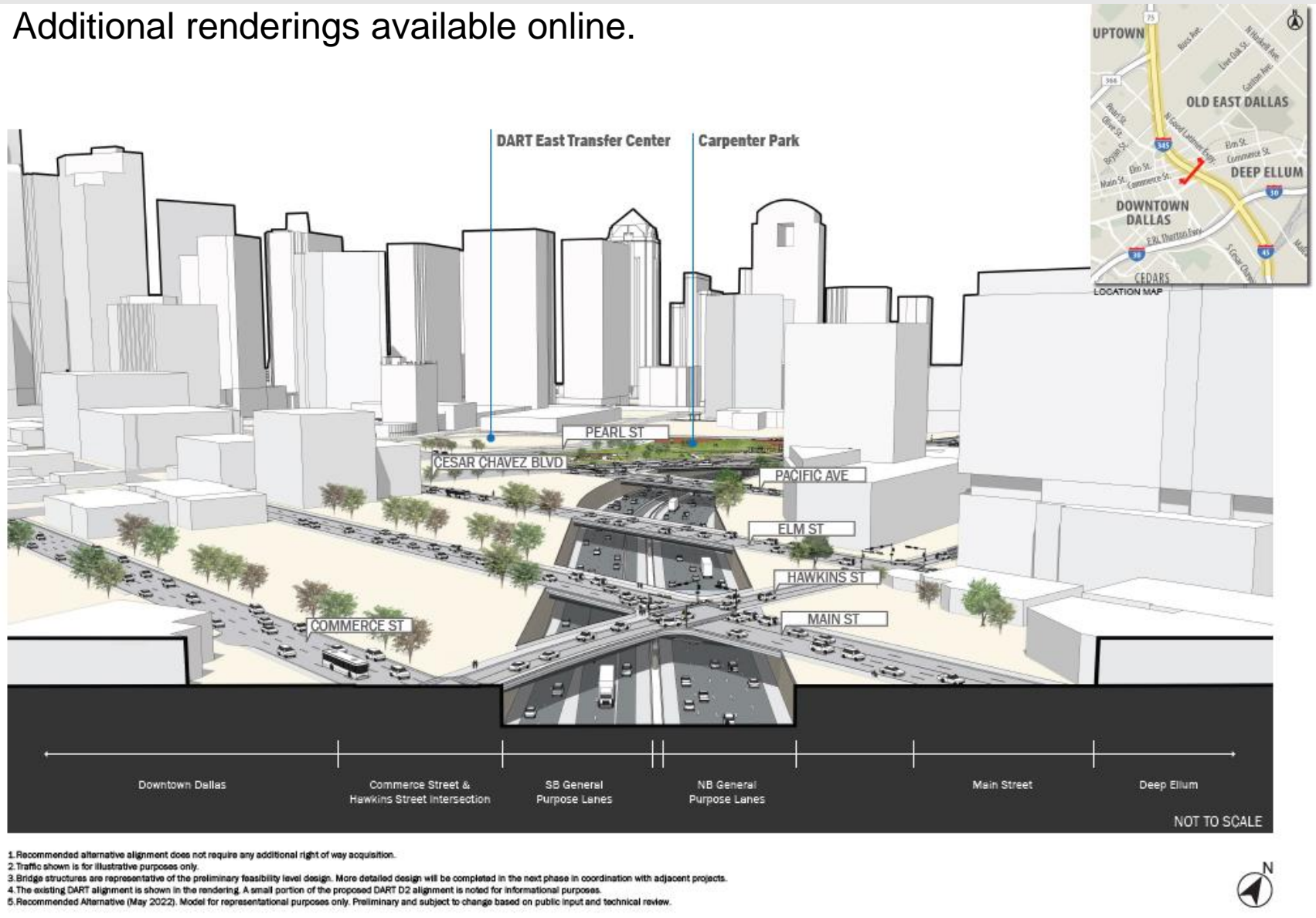
- Proposed Mainlanes
- Proposed At-Grade Roadway
- Proposed Bridge
- Proposed Ramps/Direct Connectors
- Proposed Frontage Road
- DART



Preliminary, subject to change. This is a representative rendering that was shown at the June 2021 public meetings and is included in the evaluation matrix.



Additional renderings available online.



KEY TAKEAWAY

52% of respondents supported the recommended alternative (with further study/refinements in the schematic and environmental phase) versus a removal alternative (41%). 7% of respondents preferred the No Build/Leave I-345 As-Is alternative.

POSITION LETTERS

3

**Deep Ellum Foundation
Downtown Dallas Inc.
Parks for Downtown Dallas**

PUBLIC MEETING ATTENDANCE



3,133 views online

(includes TxDOT Public Meeting Webpage view, Keepitmovingdallas.com Webpage Views and YouTube Presentation views)



104 total people

in attendance at the Sheraton Downtown and at St. Philips School and Community Center

200 TOTAL COMMENTS RECEIVED

29

of comments received at public meeting

151

of comments provided via SurveyMonkey

1

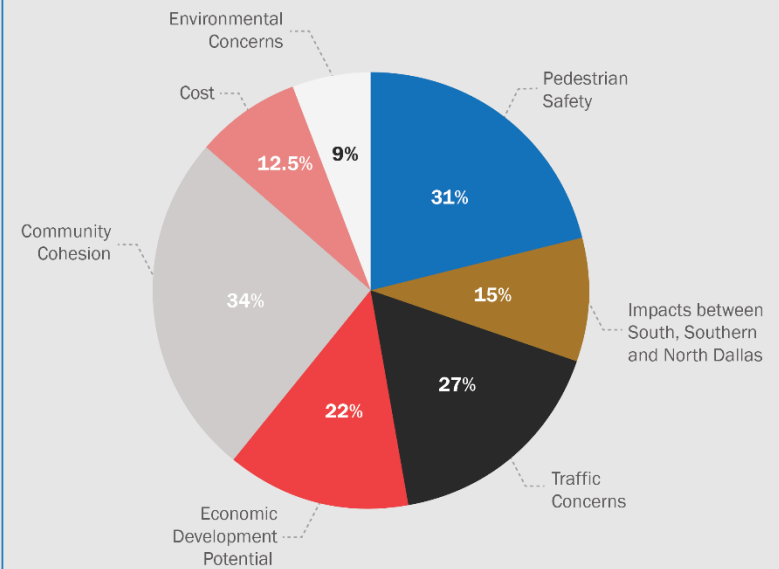
of comments received via mail

19

of email comments received

COMMON THEMES

The five common themes observed in comments from the previous meeting (June 2021) remained relevant (percentages below indicates percent of comments by theme).



Positive Feedback from the Community



“

Great compromise —
Thanks for all your
hard work.

Susan Ernst

“

The redesign is amazing. I live in Dallas
(75206) and urge TxDOT to take this on and
run with it.

Russell Daniels

“

“I commend the
thorough and
exhaustive work
done to this point.
I like the Hybrid
Alternative most as it
reconnects downtown
and Deep Ellum while
maintaining growing
traffic congestion from
southern and eastern
areas of Dallas
County.”

Matthew Lagos

“

This is the right way forward for this
project. It maintains traffic in north/south
directions while connecting downtown
and Deep Ellum.

Karen Jacobs

“

I appreciate your
willingness to work with
Dallasites to find an option
that will improve the city.

David Benners

“

Loving the hybrid option that maintains the
highway for connectivity of North/South Dallas
but allows for opportunities to cap and create
parks or business districts that connect
downtown with East Dallas. Great job!”

Chris Fisher

“

The recommended alternative is a mindful
solution to the various proposed options. This
provides both form & function and serves
vested parties needs.

Jason Hays

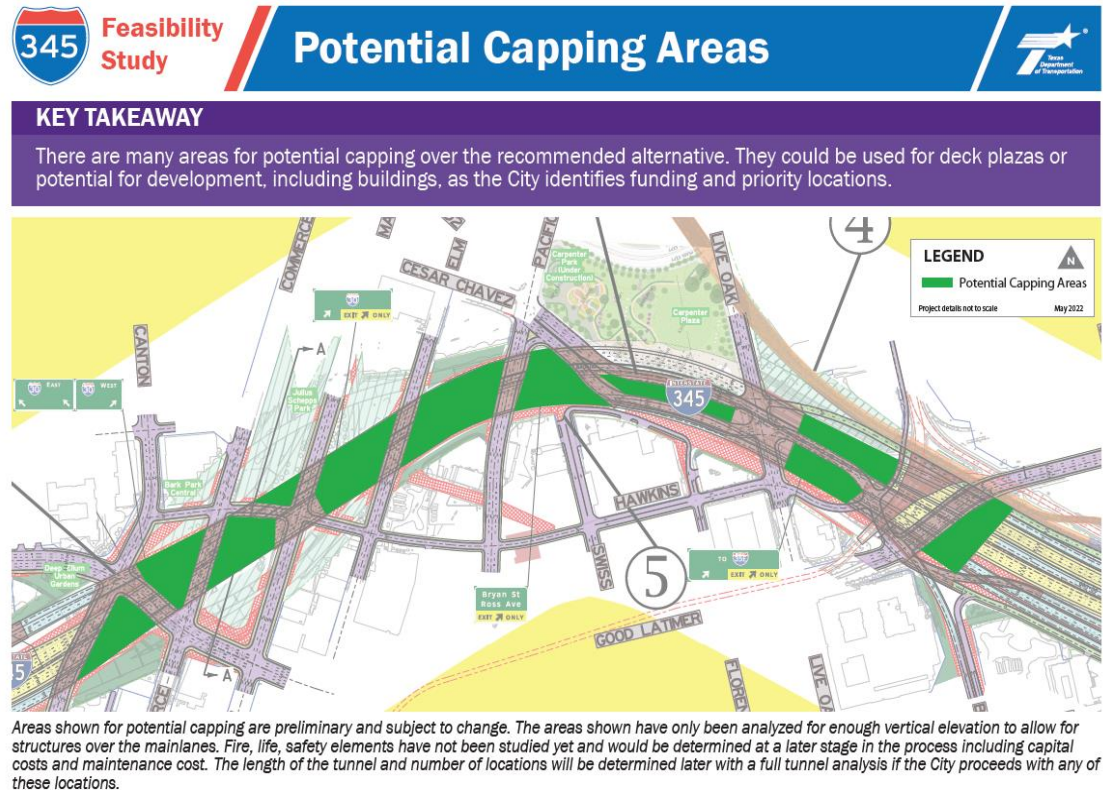
Surplus Right of Way/Potential Capping Areas



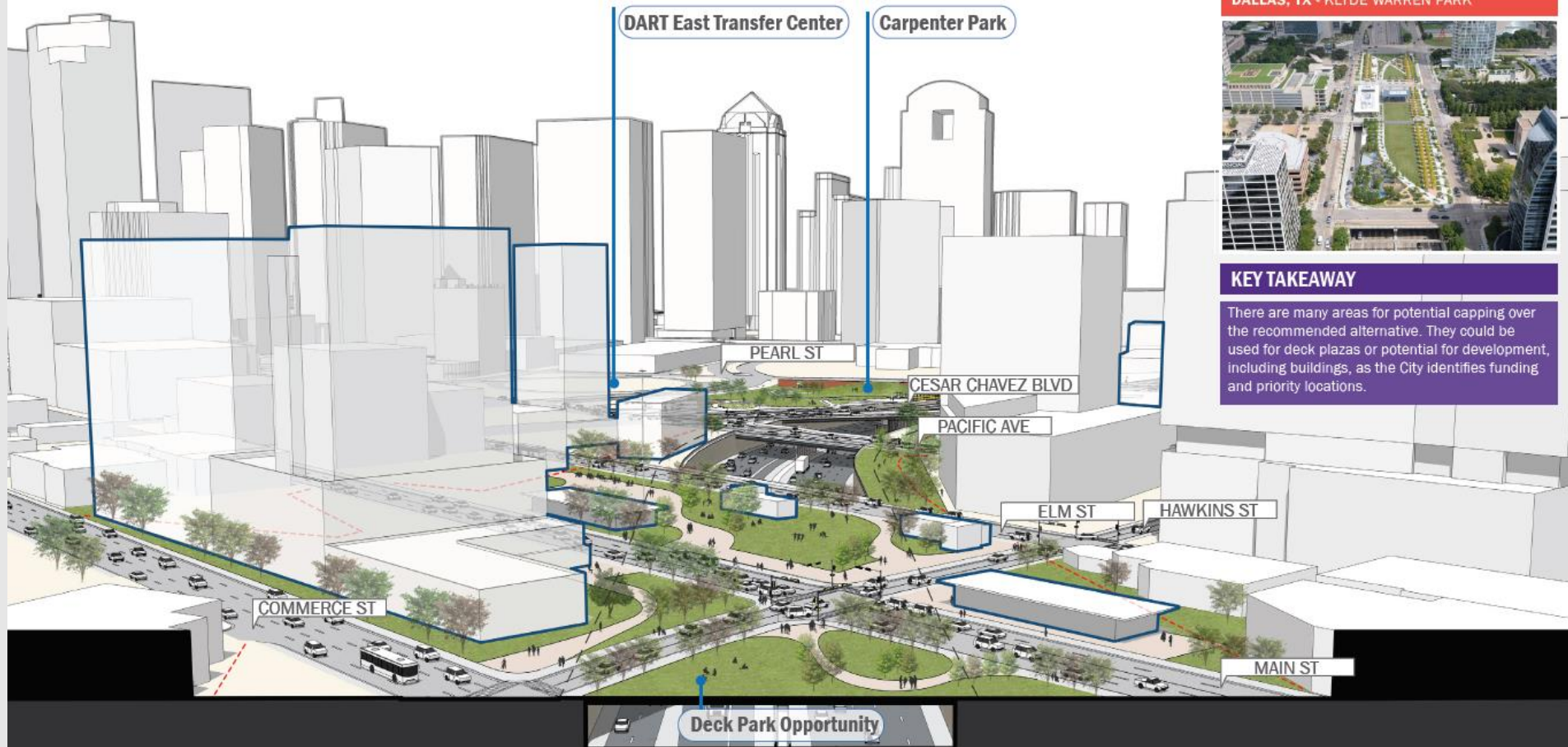
Alternative	Potential Surplus ROW (AC)	Potential Capping Area (AC)	Total Potential Development Area (AC)
Depressed	5.4	8.8	14.2
Removal	25.2	N/A	25.2
Elevated	15.2	N/A	15.2
Hybrid	8.7	9.7	18.4
Recommended	8.7	9.0	17.7

(AC) Acres

Source: Study Team (May 2022)



Potential Capping Opportunities



DALLAS, TX - KLYDE WARREN PARK



KEY TAKEAWAY

There are many areas for potential capping over the recommended alternative. They could be used for deck plazas or potential for development, including buildings, as the City identifies funding and priority locations.



----- Recommended Alternative ROW - - - - - Existing ROW ——— Potential Development

Potential Development between Elm St and Commerce St

Areas shown for potential capping are preliminary and subject to change. The areas shown have only been analyzed for enough vertical elevation to allow for structures over the mainlanes. Fire, life, safety elements have not been studied yet and would be determined at a later stage in the process including capital costs and maintenance cost. The length of the tunnel and number of locations will be determined later with a full tunnel analysis if the City proceeds with any of these locations.

Potential Capping Opportunities



DART East Transfer Center

Carpenter Park

PEARL ST

CESAR CHAVEZ BLVD

PACIFIC AVE

ELM ST

HAWKINS ST

COMMERCE ST

MAIN ST

Development Opportunity

COLUMBUS, OH - THE HIGH STREET CAP



KEY TAKEAWAY

There are many areas for potential capping over the recommended alternative. They could be used for deck plazas or potential for development, including buildings, as the City identifies funding and priority locations.

Downtown Dallas

Commerce Street &
Hawkins Street Intersection

SB General
Purpose Lanes

NB General
Purpose Lanes

Main Street

Deep Ellum

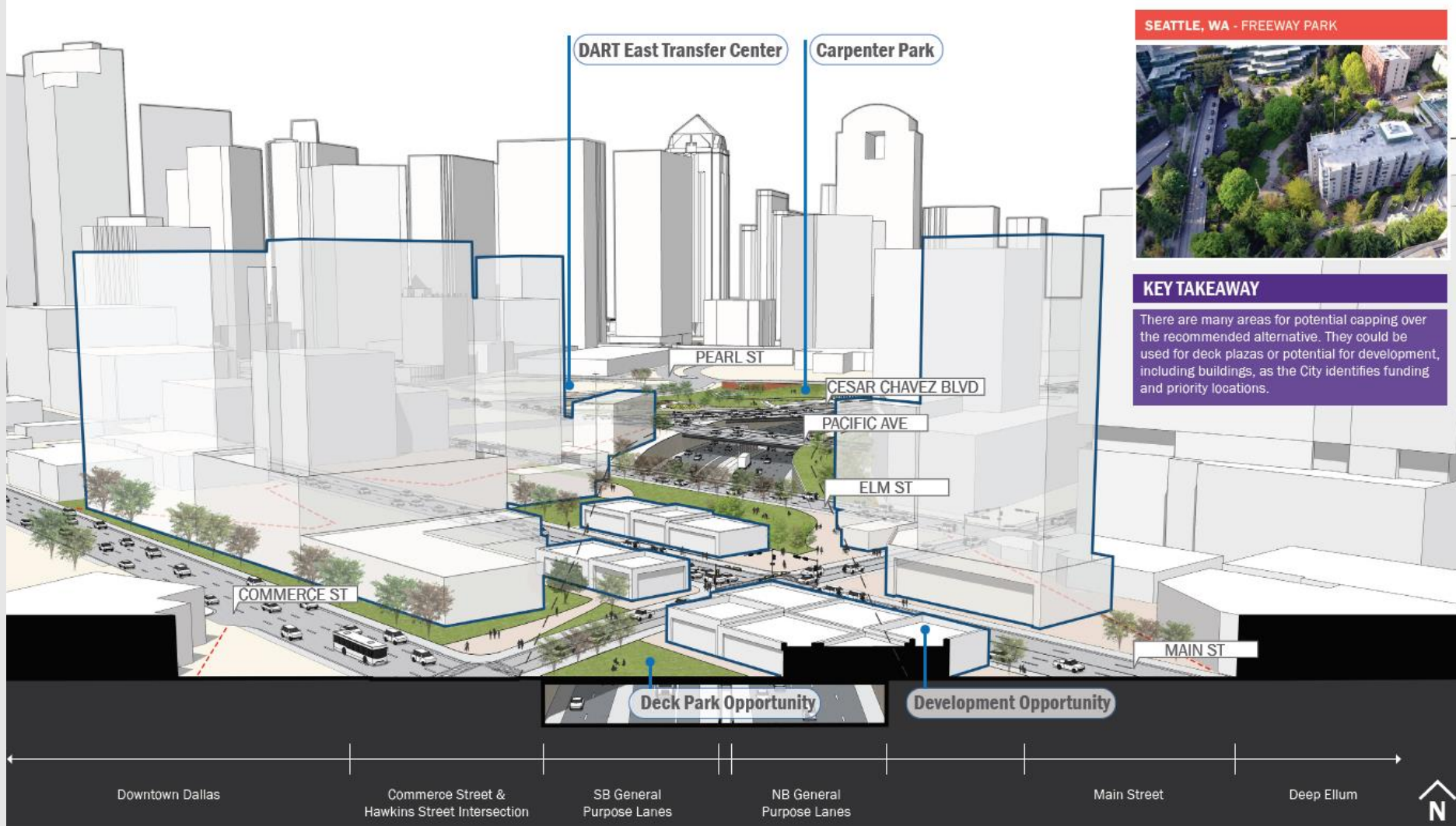


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Note: Larger version available on the project website at www.keepitmovingdallas.com/I345

TxDOT will perform bridge inspection and maintenance for estimated remaining useful service life of the bridge (approximately 25 years)

WE ARE HERE
PUBLIC MEETING #3

2022

Open to Traffic

Construction
Estimated approximately 5 years

**Ready to Let
(Project must be fully funded to let)**
"Letting" is when TxDOT notifies the construction community that a project is ready to be bid on. Construction on a project begins after the letting process is complete.

Plans, Specifications, and Estimates (PS&E)

- Construction Phasing Plans
- Striping and Signing Plans
- Drainage Details
- Traffic Signal Warrants
- Quantities/Estimates
- State Specifications
- Upon approval, contractors can bid on the project to proceed to construction (to be ready to let)

Schematic/Environmental Analysis of the Recommended Alternative

- Schematic Design
- Safety
- Traffic Operations/Level of Service
- Drainage
- Cost/Economic Impacts
- Utility Relocations
- Community Impacts/Community Cohesion
- Natural Resources Impacts
- Traffic Noise and Mitigation
- Air Quality Impacts
- Impacts to Parks and Community Facilities
- Cultural Resources
- Public Involvement

Feasibility Study Complete

The Report will be published online and include a summary of:

- Public Involvement (listening sessions, stakeholder meetings, agency coordination meetings, and public meetings (3 series))
- Alternative Analysis
- Evaluation Matrix
- Identification of Environmental Constraints
- Preliminary traffic analysis using the regional model
- Summarize process to determine the Recommended Alternative to proceed to the Schematic/Environmental Phase
- Identification of the Recommended Alternative

Timeline theoretical as many factors can change design and schedule



- Development of I-345 to be aligned with various planning documents including:
 - CityMap and the 360 Plans
 - Dallas Strategic Mobility Plan (DSMP)
 - Vision Zero and the Bike Plans
 - Complete Streets Design Manual (2016)
 - Comprehensive Environmental & Climate Action Plan (CECAP)
 - Comprehensive Housing Policy (2018)
 - Parks and Recreation Comprehensive Plan (2016)
 - Other Internal and External Plans
- The DSMP's *Driving Principles* that guide investment and policy decisions include: Safety, Economic Vitality, Environmental Sustainability, Housing, Equity and Innovation

Questions?